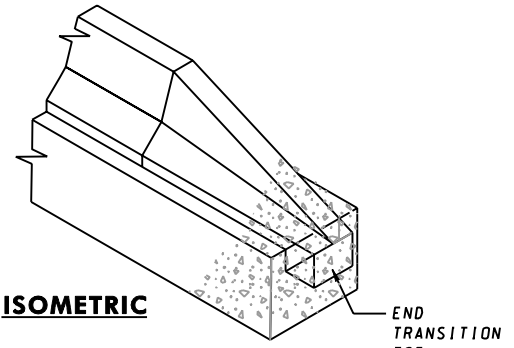
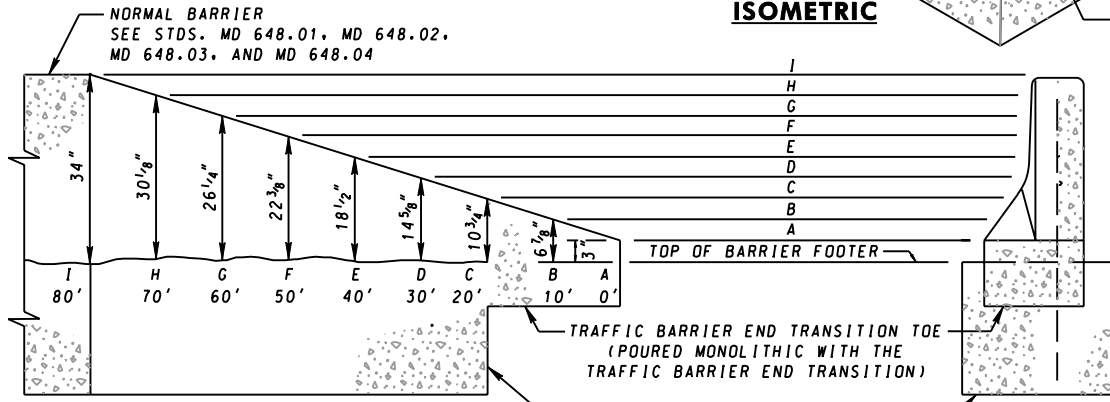


PLAN

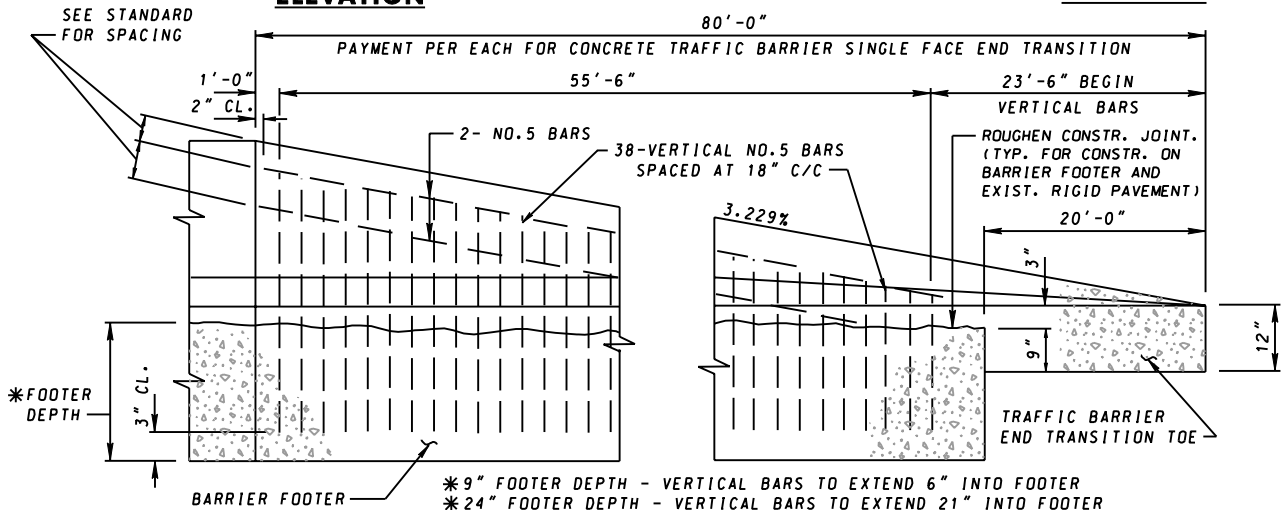


ISOMETRIC



ELEVATION

END VIEW



REINFORCEMENT STEEL DETAIL

NOTES

1. THE TRAFFIC BARRIER END TRANSITION AND BARRIER FOOTER SHALL BE CONSTRUCTED USING THE FIXED FORM METHOD. SEE STDS. MD 648.01, MD 648.02, AND MD 648.03 FOR BARRIER FOOTER DETAILS. THE BARRIER AND FOOTER SHALL BE CAST SEPARATELY. MONOLITHIC PLACEMENT NOT PERMITTED.
2. THE TRAFFIC BARRIER END TRANSITION AND BARRIER FOOTER SHALL BE CONSTRUCTED USING CONCRETE MIX NO.6 (4500 PSI).
3. ALL REINFORCEMENT BARS, INCLUDING ENDS, AND TIES SHALL BE EPOXY COATED. ALL BAR LAPS TO BE 30 BAR DIAMETERS. TIE BARS TOGETHER.
4. THIS TRAFFIC BARRIER END TRANSITION IS PROHIBITED WHEN THE DESIGN SPEED IS 45 MPH OR GREATER. THE ENGINEER MUST DETERMINE THE TYPE OF END TREATMENT REQUIRED FOR DESIGN SPEEDS OVER 45 MPH.
5. THE COST OF THE BARRIER END TRANSITION TOE, REINFORCEMENT, DRILLED HOLES, GROUT, LABOR, TOOLS, EQUIPMENT, ETC., SHALL BE INCIDENTAL TO THE CONTRACT UNIT PRICE PER EACH FOR THE CONCRETE TRAFFIC BARRIER SINGLE FACE END TRANSITION.
6. TOLERANCES IN DIMENSIONS SHOWN SHALL BE WITHIN 1/4".

JERSEY SHAPE - FOR REPLACEMENT PURPOSES ONLY

SPECIFICATION 604	CATEGORY CODE ITEMS
APPROVED	<i>Kirk G. McCall</i> DIRECTOR - OFFICE OF HIGHWAY DEVELOPMENT
	APPROVAL • SHA REVISIONS
	APPROVAL 7-16-90
	REVISD 10-1-01
	REVISD
	REVISD
APPROVAL • FEDERAL HIGHWAY ADMINISTRATION	APPROVAL 7-31-90
	REVISD 3-28-01
	REVISD
	REVISD

Maryland Department of Transportation
STATE HIGHWAY ADMINISTRATION
 STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES
CONCRETE JERSEY SHAPE TRAFFIC BARRIER
SINGLE FACE END TRANSITION

STANARD NO. MD 648.10