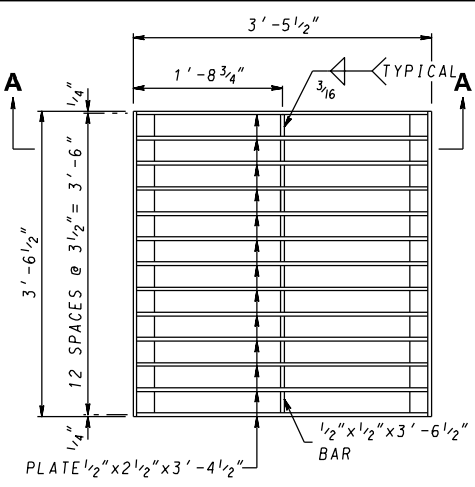
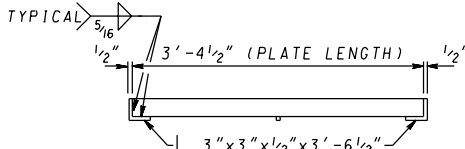


**GENERAL NOTES**

1. CONCRETE TO BE MIX NO.2 (3,000 PSI)
2. GRATE IS TO BE OF STEEL CONSTRUCTION & SHALL BE SQUARE, FLAT & TRUE.
3. STRUCTURAL STEEL SHALL BE A.S.T.M. DESIGNATION A-36.
4. GRATE TO BE GALV. AFTER FABRICATION IN ACCORDANCE WITH ASTM. DESIGNATION A-123.
5. SEE LATEST S.H.A. SPECIFICATIONS.
6. THIS INLET IS TO BE USED IN MEDIAN DITCHES AND ANY DITCH BEYOND THE SHOULDER AREA. THIS INLET IS NOT TO BE USED IN ROADWAY OR SHOULDER PAVEMENT AREAS OR AREAS WHERE BICYCLE OR MOTORCYCLE TRAFFIC IS ANTICIPATED.
7. WHEN DEPTH IS LESS THAN 7'-0", WALL REINFORCEMENT SHALL BE ONE LAYER OF NO. 4 DEFORMED BARS @ 6" C/C, TWO WAYS, AND HAVE 3" COVER ON INSIDE. WHEN DEPTH IS GREATER THAN 7'-0" AND LESS THAN 15'-0", WALL REINFORCEMENT TO BE TWO LAYERS OF NO. 4 DEFORMED BARS @ 6" C/C, TWO WAYS, ON INSIDE AND OUTSIDE OF WALL WITH 2" COVER.
8. BASE REINFORCEMENT SHALL BE ONE LAYER OF NO. 4 DEFORMED BARS @ 6" C/C, TWO WAYS, WITH 2" COVER FROM TOP OF BASE.
9. INLET HAS BEEN DESIGNED FOR HS-25 LOADING ACCORDING TO AASHTO LRFD FOR MAXIMUM DEPTH OF 15'-0"



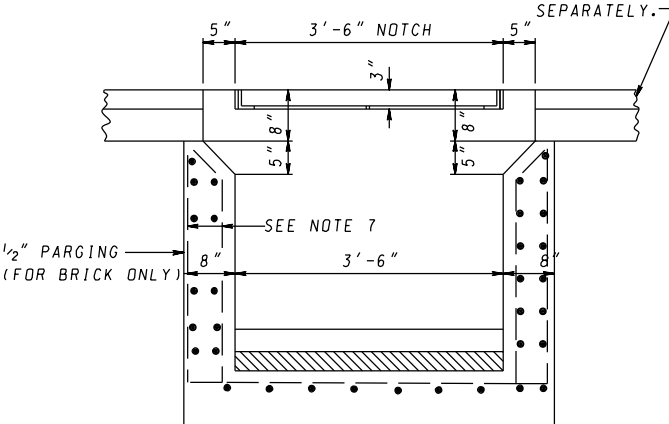
**PLAN**



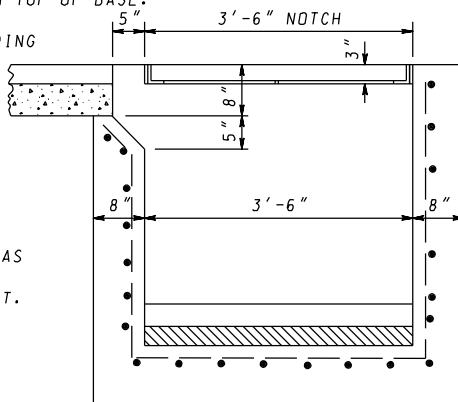
**SECTION A-A**

NOTE: GRATE TO BE AS SHOWN OR FURNISH APPROVED EQUIVALENT.

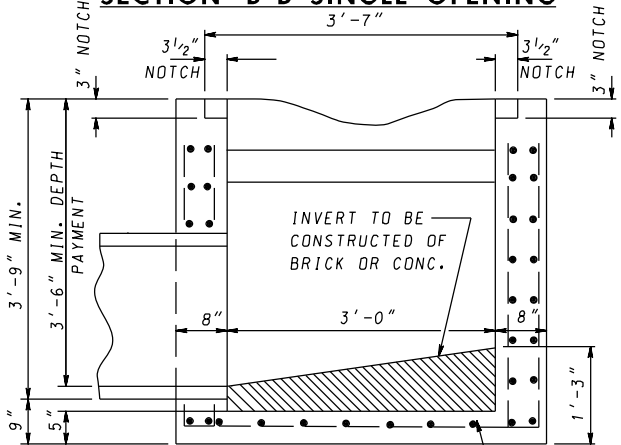
CONCRETE CUTTER TO BE PAID FOR SEPARATELY.



**SECTION B-B DOUBLE OPENING**

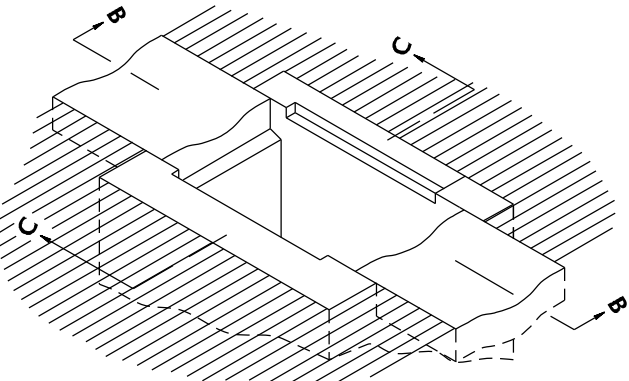


**SECTION B-B SINGLE OPENING**

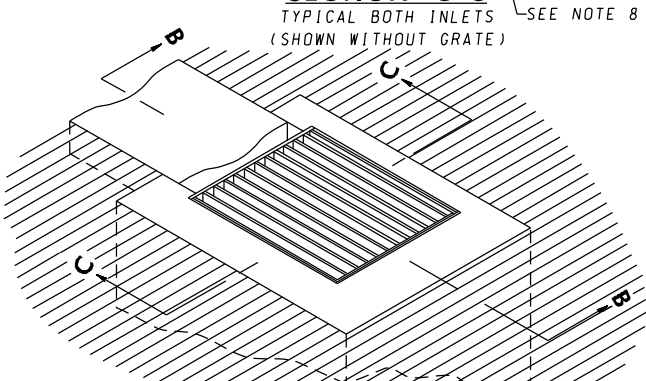


**SECTION C-C**

TYPICAL BOTH INLETS (SHOWN WITHOUT GRATE) SEE NOTE 8



**DOUBLE OPENING**  
(SHOWN WITHOUT GRATE)



**SINGLE OPENING**

SPECIFICATION <b>305</b>	CATEGORY CODE ITEMS
APPROVED	DIRECTOR - OFFICE OF HIGHWAY DEVELOPMENT
	APPROVAL • SHA REVISIONS
	APPROVAL • FEDERAL HIGHWAY ADMINISTRATION
	APPROVAL 5-28-82
	REVISD 10-1-01
	REVISD 10-7-14

**Maryland Department of Transportation**  
**STATE HIGHWAY ADMINISTRATION**  
 STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES

**STANDARD SINGLE OR DOUBLE OPENING**  
**TYPE K INLET OPEN - END GRATE**

**STANDARD NO. MD 378.05**