






# TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION

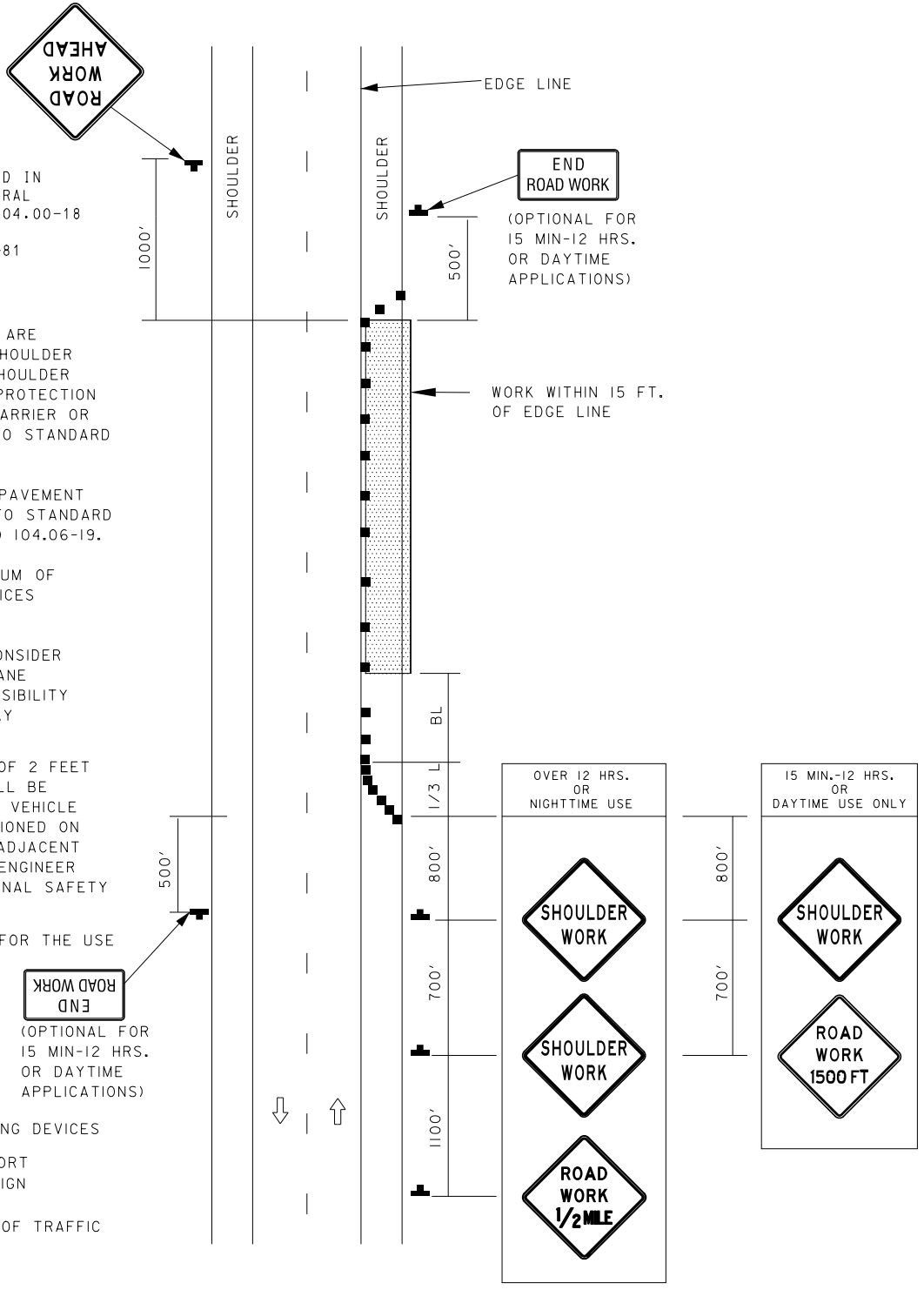
**IMPORTANT:**  
THIS DRAWING SHALL BE USED IN COMBINATION WITH THE GENERAL NOTES MD 104.00-01 - MD 104.00-18 AND STANDARD DETAILS MD 104.01-01 - MD 104.01-81

**NOTES:**


1. SHOULDER CLOSED SIGNS ARE REQUIRED IN PLACE OF SHOULDER WORK SIGNS WHEN THE SHOULDER IS CLOSED BY POSITIVE PROTECTION (TEMPORARY CONCRETE BARRIER OR SIMILAR DEVICE). REFER TO STANDARD NO. MD 104.06-18.
2. WHEN WORK INVOLVES A PAVEMENT EDGE DROP-OFF, REFER TO STANDARD NOS. MD 104.06-15 TO MD 104.06-19.
3. THERE SHALL BE A MINIMUM OF SEVEN CHANNELIZING DEVICES IN THE SHOULDER TAPER.
4. THE ENGINEER SHOULD CONSIDER ADDITIONAL, ADJACENT LANE CLOSURES WHEN THE POSSIBILITY OF UNPLANNED TRAVELWAY ENCROACHMENTS EXISTS.
5. FOR SAFETY, A MINIMUM OF 2 FEET LATERAL CLEARANCE SHALL BE MAINTAINED BETWEEN ANY VEHICLE AND/OR EQUIPMENT POSITIONED ON THE SHOULDER AND THE ADJACENT OPEN TRAVEL LANE. THE ENGINEER SHOULD CONSIDER ADDITIONAL SAFETY MEASURES.
6. REFER TO MD 104.01-11A FOR THE USE OF A PV.
7. REFER TO MD 104.01-30C FOR THE POSITIONING OF A PV.

**KEY:**

-  CHANNELIZING DEVICES
-  SIGN SUPPORT
-  FACE OF SIGN
-  DIRECTION OF TRAFFIC
-  WORK SITE



<b>SPECIFICATION</b> 104	<b>CATEGORY CODE ITEMS</b>		
<b>APPROVED</b>	<i>Cedric Wald</i> DIRECTOR - OFFICE OF TRAFFIC AND SAFETY		
<b>APPROVAL SHA REVISIONS</b>	<b>APPROVAL FEDERAL HIGHWAY ADMINISTRATION</b>		
<b>APPROVAL</b>	8-20-03	<b>APPROVAL</b>	9-23-03
<b>REVISED</b>	8-11-10	<b>REVISED</b>	10-5-10
<b>REVISED</b>	2-19-24	<b>REVISED</b>	11-16-23
<b>REVISED</b>		<b>REVISED</b>	



**MARYLAND DEPARTMENT OF TRANSPORTATION**  
STATE HIGHWAY ADMINISTRATION

**STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES**  
**SHOULDER WORK /2-LANE, 2-WAY**  
**GREATER THAN 40 MPH**

**STANDARD NO. MD 104.02-01**