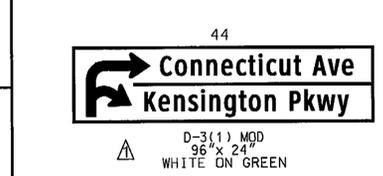
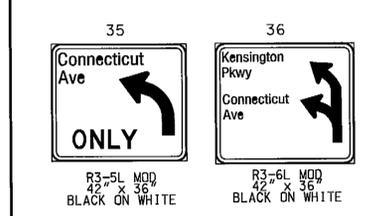
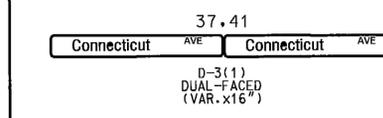
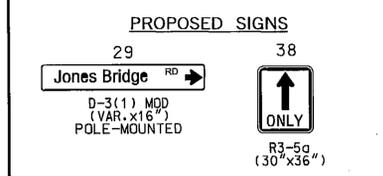
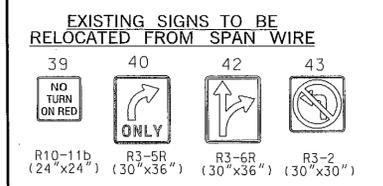


MD 185 IS ASSUMED TO RUN IN A NORTH-SOUTH DIRECTION



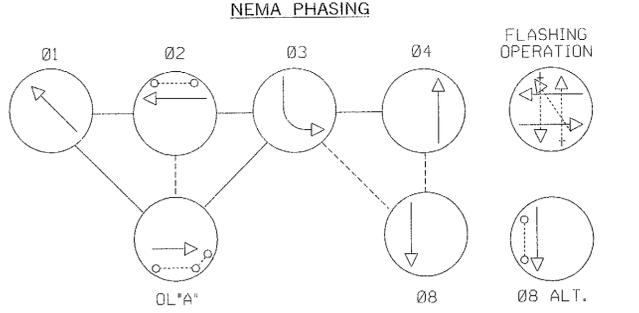
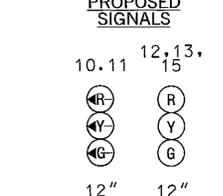
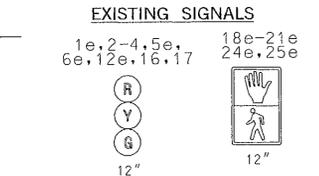
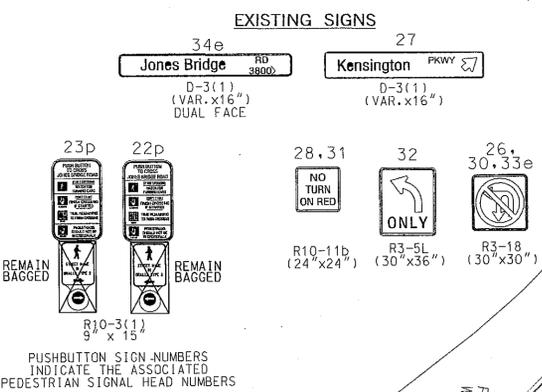
NOTE: SIGN DETAIL FOR SIGNS 35 AND 36 SHALL BE SHOWN ON PLAN SHEET SG-04.2

CONSTRUCTION DETAILS

- INSTALL LED TRAFFIC SIGNAL HEADS, VIDEO DETECTION CAMERAS, AND SIGNS ON THE EXISTING SIGNAL POLE MAST ARM PRIOR TO THE START OF PHASE 2A CONSTRUCTION. INSTALL SIGNAL HEADS AND SIGNS; CONNECT SIGNAL HEADS TO THE EXISTING WIRING.
- TEMPORARY STOP LINE (SEE TRAFFIC CONTROL PLAN).
- TEMPORARY CROSSWALK (SEE TRAFFIC CONTROL PLAN).
- INSTALL EXISTING SIGNS FROM SPAN WIRE ONTO THE EXISTING SIGNAL POLE MAST ARM.
- TEMPORARY PAVEMENT MARKING ARROW AND LEGEND (SEE TRAFFIC CONTROL PLAN).
- DURING THIS PHASE OF CONSTRUCTION THIS PEDESTRIAN RAMP SHALL BE CLOSED TO PEDESTRIAN TRAFFIC.
- REMOVE THE WOOD POLE AND ASSOCIATED EQUIPMENT. RELOCATE OVERHEAD SIGNS FROM THE SPAN WIRE TO THE EXISTING SIGNAL POLE MAST ARM "D".
- REMOVE EXISTING WIRING FROM EXISTING SIGNAL POLE AND HANDHOLE PRIOR TO THE RECONSTRUCTION OF THE MEDIAN. SIGNAL POLE HANDHOLE SHALL BE REMOVED DURING THIS PHASE OF RECONSTRUCTION.
- PEDESTRIAN PUSHBUTTON SHALL REMAIN BAGGED UNTIL SIGNAL IS FULLY OPERATIONAL.

GEOMETRIC LEGEND	
---	EXISTING
---	PROPOSED

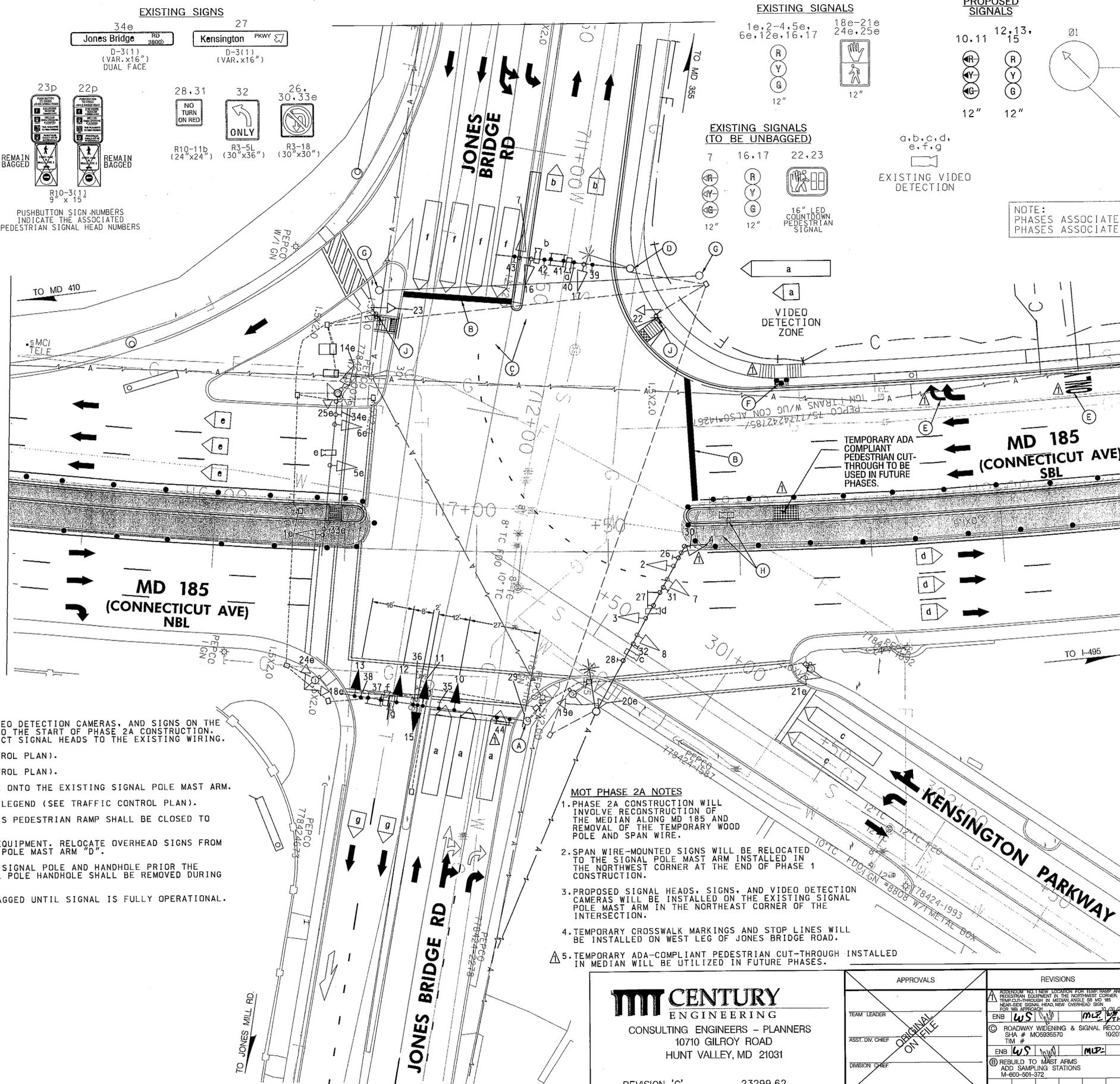
UTILITY LEGEND	
SD	STORM DRAIN
G	GAS MAIN
W	WATER MAIN
S	SEWER MAIN
E	ELECTRIC CABLES
A	AERIAL CABLES
T	TELEPHONE CABLES
F	FIBER-OPTIC



NOTE: PHASES ASSOCIATED BY A DASHED LINE WILL OPERATE CONCURRENTLY. PHASES ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.

GENERAL NOTES

- THE CONTRACTOR SHALL VERIFY ALL UNDERGROUND UTILITIES PRIOR TO INSTALLING PROPOSED SIGNAL EQUIPMENT. IF ANY UTILITY CONFLICTS SHOULD ARISE, THE CONTRACTOR SHALL CONTACT THE PROJECT ENGINEER.
- ALL TRAFFIC SIGNAL FOUNDATIONS SHALL BE INSTALLED AT THE FINAL SIDEWALK OR CURB GRADE FOR CLOSED SECTIONS, AND THE HIGHEST ROADWAY PROFILE GRADE FOR OPEN SECTIONS TO MEET CLEARANCES AS SPECIFIED IN MD STD. 816.03, MD STD. 818.01, MD 818.02, AND MD STD. 818.04. THE CONTRACTOR SHALL VERIFY ULTIMATE GRADES PRIOR TO THE INSTALLATION OF ALL PROPOSED SIGNAL EQUIPMENT.
- PUSHBUTTONS ARE TO BE LOCATED SO THAT THEY CAN BE ACTIVATED BY A PERSON IN A WHEELCHAIR REACHING LESS THAN 18" FROM A 60" x 60" LEVEL LANDING AREA WITH A CROSS SLOPE OF LESS THAN OR EQUAL TO 2%.
- LOCATION OF ACCESSIBLE PEDESTRIAN SIGNAL PUSHBUTTONS MUST MEET LOCATION REQUIREMENTS OF MUTCD SEC. 4E.09 AND FIG. 4E-2 AND THE NCHRP PUBLICATION, "ACCESSIBLE PEDESTRIAN SIGNALS: GUIDE TO BEST PRACTICE". IF NOT MET, THE CONTRACTOR IS TO STOP WORK ON PUSHBUTTON LOCATIONS UNTIL A DESIGN WAIVER IS OBTAINED AND APPROVED BY THE DIRECTOR OF THE OFFICE OF TRAFFIC AND SAFETY.
- PROPOSED SIGNAL EQUIPMENT SHALL BE INSTALLED PRIOR TO THE CONSTRUCTION OF THE SIDEWALK AND PEDESTRIAN RAMPS, AND THE INSTALLATION OF DETECTABLE WARNING SURFACES.
- THE CONTRACTOR IS RESPONSIBLE FOR REMOVING UNUSED CABLES FROM EXISTING HANDHOLES AND CONDUITS UTILIZED FOR REVISION 'C'.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR TERMINATING ALL SIGNAL CABLES TO THE APPROPRIATE TERMINALS AND FOR PROPERLY LABELING EACH CABLE.
- VIDEO CAMERA LOCATION/ALIGNING SHALL BE COORDINATED WITH THE SHA ENGINEER.
- THE 10' SEPARATION BETWEEN PUSHBUTTONS IS TO BE MEASURED FROM FACE OF PUSHBUTTON TO FACE OF PUSHBUTTON, NOT CENTER TO CENTER OF POLE.
- PUSHBUTTON ARROWS ARE TO BE PARALLEL TO THE CROSSING FOR WHICH THEY ARE INTENDED.
- FOR FINAL PAVEMENT MARKINGS, REFER TO THE PAVEMENT MARKING PLAN, OTHER THAN THOSE DETAILED ON THIS PLAN. ALL PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH MD SHA STANDARDS.
- ALL EXISTING TRAFFIC SIGNAL EQUIPMENT REMOVED SHALL BECOME THE PROPERTY OF THE SIGNAL CONTRACTOR UPON COMPLETION OF THE NEW SIGNAL.



MOT PHASE 2A NOTES

- PHASE 2A CONSTRUCTION WILL INVOLVE RECONSTRUCTION OF THE MEDIAN ALONG MD 185 AND REMOVAL OF THE TEMPORARY WOOD POLE AND SPAN WIRE.
- SPAN WIRE-MOUNTED SIGNS WILL BE RELOCATED TO THE SIGNAL POLE MAST ARM INSTALLED IN THE NORTHWEST CORNER AT THE END OF PHASE 1 CONSTRUCTION.
- PROPOSED SIGNAL HEADS, SIGNS, AND VIDEO DETECTION CAMERAS WILL BE INSTALLED ON THE EXISTING SIGNAL POLE MAST ARM IN THE NORTHEAST CORNER OF THE INTERSECTION.
- TEMPORARY CROSSWALK MARKINGS AND STOP LINES WILL BE INSTALLED ON WEST LEG OF JONES BRIDGE ROAD.
- TEMPORARY ADA-COMPLIANT PEDESTRIAN CUT-THROUGH INSTALLED IN MEDIAN WILL BE UTILIZED IN FUTURE PHASES.

PHASE 2A

STATE OF MARYLAND
DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION
OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION
BRAC INTERSECTION IMPROVEMENTS
MD 185 (CONNECTICUT AVENUE) AT
JONES BRIDGE ROAD AND KENSINGTON PKWY.
BETHESDA, MD

MOT PHASE 2A - TRAFFIC SIGNAL PLAN

SCALE 1" = 20'. ADVERTISED DATE: JUNE 1990. CONTRACT NO. BW-339-902-312

DESIGNED BY: R. J. MILSTEAD COUNTY: MONTGOMERY
DRAWN BY: J. Y. ASHCRAFT LOGMILE: 15018502.22
CHECKED BY: _____ TMS NO. _____
F.A.P. NO. SEE TITLE SHEET TOD NO. _____

TS NO. 2649C DRAWING SG-03.2 OF 12 SHEET NO. 103 OF 123

CENTURY ENGINEERING
CONSULTING ENGINEERS - PLANNERS
10710 GILROY ROAD
HUNT VALLEY, MD 21031

REVISION 'C' 23299.62

APPROVALS	
TEAM LEADER	ASST. DIV. CHIEF
DIVISION CHIEF	OFFICE DIRECTOR

REVISIONS	
ENB	LS
ENB	LS
ENB	LS