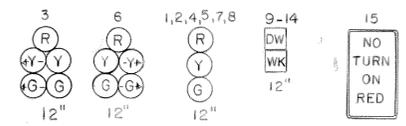
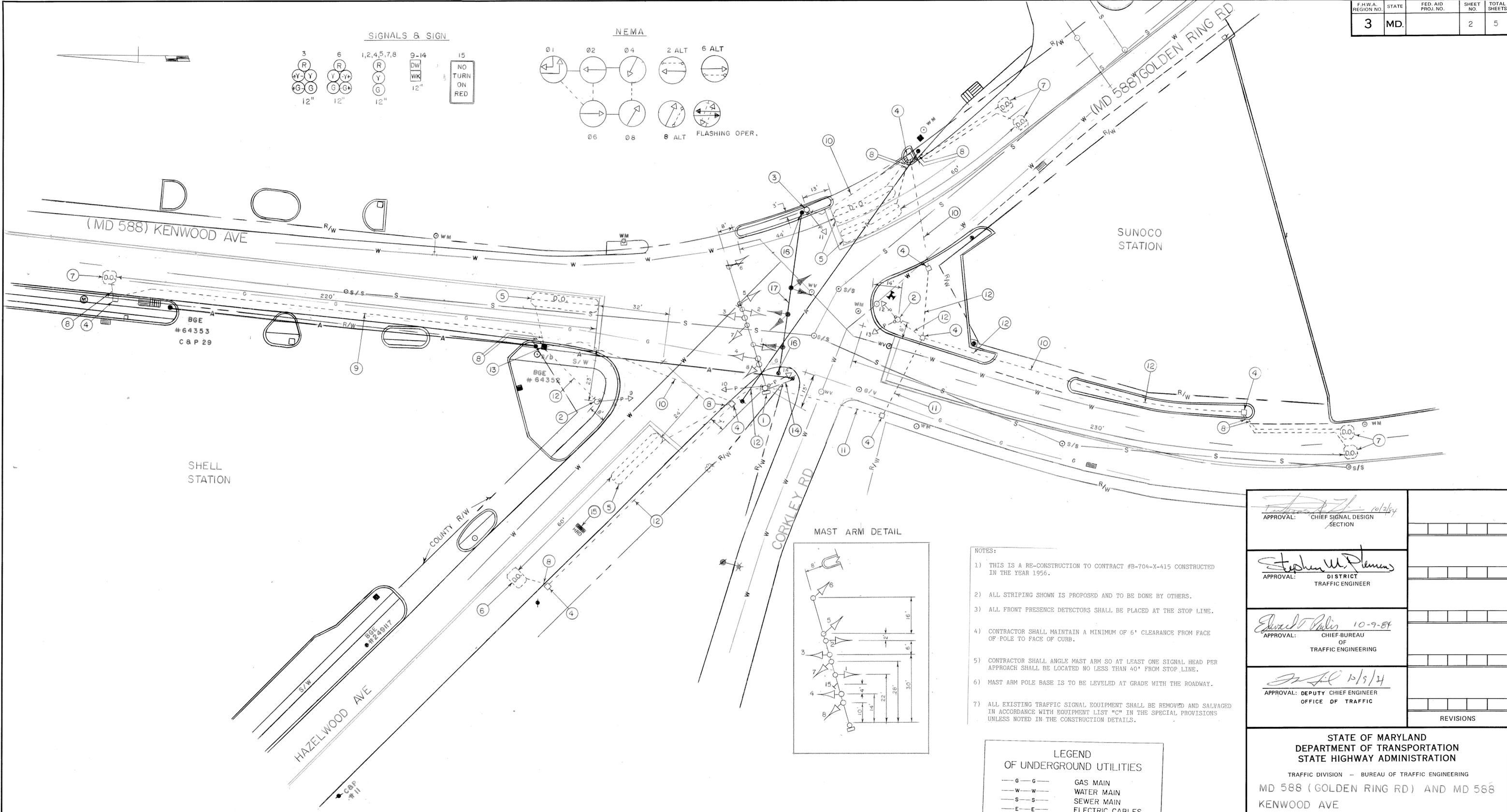
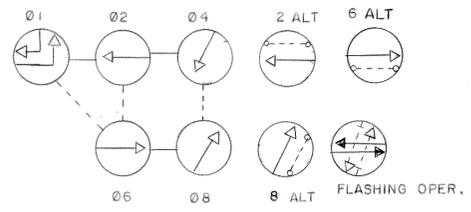


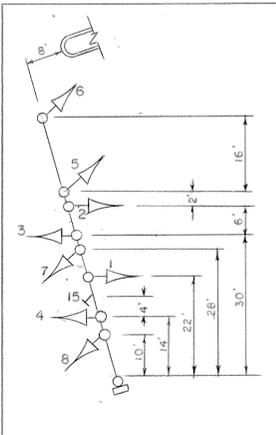
SIGNALS & SIGN



NEMA



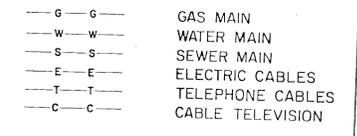
MAST ARM DETAIL



NOTES:

- 1) THIS IS A RE-CONSTRUCTION TO CONTRACT #B-704-X-415 CONSTRUCTED IN THE YEAR 1956.
- 2) ALL STRIPING SHOWN IS PROPOSED AND TO BE DONE BY OTHERS.
- 3) ALL FRONT PRESENCE DETECTORS SHALL BE PLACED AT THE STOP LINE.
- 4) CONTRACTOR SHALL MAINTAIN A MINIMUM OF 6' CLEARANCE FROM FACE OF POLE TO FACE OF CURB.
- 5) CONTRACTOR SHALL ANGLE MAST ARM SO AT LEAST ONE SIGNAL HEAD PER APPROACH SHALL BE LOCATED NO LESS THAN 40' FROM STOP LINE.
- 6) MAST ARM POLE BASE IS TO BE LEVELED AT GRADE WITH THE ROADWAY.
- 7) ALL EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED AND SALVAGED IN ACCORDANCE WITH EQUIPMENT LIST "C" IN THE SPECIAL PROVISIONS UNLESS NOTED IN THE CONSTRUCTION DETAILS.

LEGEND OF UNDERGROUND UTILITIES



<i>[Signature]</i> APPROVAL: CHIEF SIGNAL DESIGN SECTION	
<i>[Signature]</i> APPROVAL: DISTRICT TRAFFIC ENGINEER	
<i>[Signature]</i> 10-9-84 APPROVAL: CHIEF BUREAU OF TRAFFIC ENGINEERING	
<i>[Signature]</i> 10/9/84 APPROVAL: DEPUTY CHIEF ENGINEER OFFICE OF TRAFFIC	
REVISIONS	

STATE OF MARYLAND
DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

TRAFFIC DIVISION - BUREAU OF TRAFFIC ENGINEERING

MD 588 (GOLDEN RING RD) AND MD 588
KENWOOD AVE

SCALE: 1" = 20'	DATE: 7/11/84	CONTRACT NO.: B-768-501-485
DRAWN BY: D MILLER	COUNTY: BALTIMORE	
DESIGNED BY: DCD	SIGNAL NO.: 03063	
CHECKED BY: <i>[Signature]</i> 10/5/84	DRAWING NO.: TS-1965	